

MD 157 IS ASSUMED TO RUN
IN A NORTH-SOUTH DIRECTION

EXISTING LED SIGNALS
TO REMAIN

1,4,5
6,7,8

R
Y
G
12"

EXISTING LED SIGNALS
TO BE RELOCATED

2,3

R
Y
G
12"

PROPOSED LED SIGNALS

9,10,11,
12,13,14

16" LED
COUNTDOWN
PEDESTRIAN
SIGNAL

15a,15b,17a,17b

Searles RD
DUAL FACED

EXISTING SIGNS TO REMAIN

20

Merritt BLVD
North 157 South

23

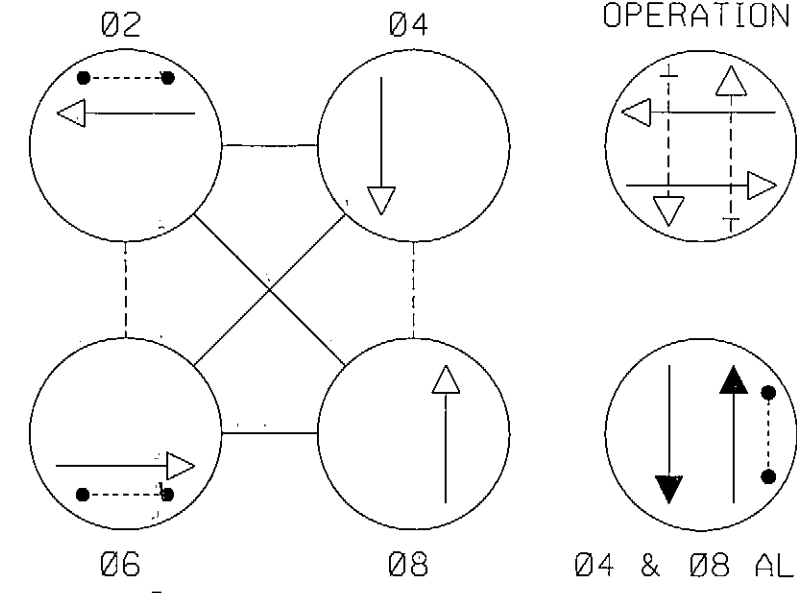
Merritt BLVD
South 157 North

PROPOSED VIDEO DETECTION

a,b,c,d

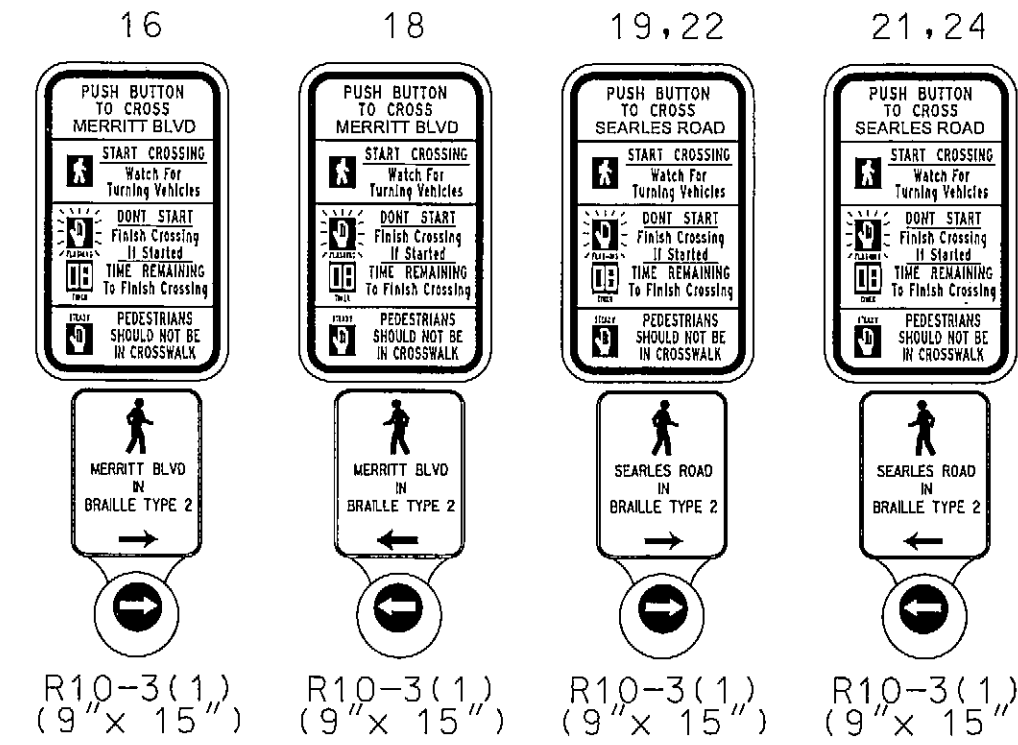
NEMA PHASING

FLASHING
OPERATION



NOTE:
PHASES ASSOCIATED BY A DASHED LINE MAY/WILL OPERATE CONCURRENTLY.
PHASES ASSOCIATED BY A SOLID LINE WILL NOT OPERATE CONCURRENTLY.

PROPOSED ACCESSIBLE PUSHBUTTONS AND SIGNS



EX. UG IC SOUTH
TO MEADOW LN/MOORGATE RD

MD 157 (MERRITT BOULEVARD) NBR

MD 157 (MERRITT BOULEVARD) SBR

EX OH IC NORTH
TO HOLABIRD AVE/WISE AVE

TO MD 151

CONSTRUCTION DETAILS

- INSTALL A 10 FT. BREAKAWAY PEDESTAL POLE WITH FOUNDATION SHA STD. MD 801.01-01, BREAKAWAY COUPLINGS, LED COUNTDOWN PEDESTRIAN SIGNAL HEAD, ACCESSIBLE PUSHBUTTON (ARROW LEFT) AND SIGN R10-3(1) "PUSH BUTTON TO CROSS MERRITT BLVD" (NOTE: 1-2 IN. 90 DEGREE PVC BEND).
- INSTALL LED COUNTDOWN PEDESTRIAN SIGNAL HEAD, ACCESSIBLE PUSHBUTTON (ARROW RIGHT) AND SIGN R10-3(1) "PUSH BUTTON TO CROSS SEARLES ROAD" ON EXISTING SIGNAL POLE.
- INSTALL A 10 FT. BREAKAWAY PEDESTAL POLE WITH FOUNDATION SHA STD. MD 801.01-01, BREAKAWAY COUPLINGS, LED COUNTDOWN PEDESTRIAN SIGNAL HEAD, ACCESSIBLE PUSHBUTTON (ARROW RIGHT) AND SIGN R10-3(1) "PUSH BUTTON TO CROSS SEARLES ROAD" (NOTE: 1-2 IN. 90 DEGREE PVC BEND).
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- INSTALL 2 IN. SCHEDULE 80 RIGID PVC ELECTRICAL CONDUIT - TRENCHED.
- USE EXISTING HANDHOLE AND ADJUST TO GRADE.
- USE EXISTING CONDUIT.
- INSTALL 12 IN. WHITE PREFORMED THERMOPLASTIC PAVEMENT MARKING LINE FOR CROSSWALK. (SEE NOTES 9 AND 10).
- INSTALL 24 IN. WHITE PREFORMED THERMOPLASTIC PAVEMENT MARKING LINE FOR STOPLINE. (SEE NOTE 9).
- ADJUST NON-INVASIVE MICROLOOP PROBE SETS AS NECESSARY TO BE CENTERED IN TRAVEL LANES.
- INSTALL VIDEO DETECTION CAMERA ON EXISTING MAST ARM.
- REMOVE EXISTING VIDEO DETECTION CAMERA AND INSTALL NEW VIDEO DETECTION CAMERA ON EXISTING LIGHTING ARM.
- RELOCATE EXISTING SIGNAL HEAD ON EXISTING MAST ARM (SEE MAST ARM DETAIL NEXT SHEET).

ADJUST ONE HANDHOLE
TO GRADE WITHIN
BREAK AREA

GENERAL NOTES CONTINUED

- PUSHBUTTONS ARE TO BE LOCATED SO THAT THEY CAN BE ACTIVATED BY A PERSON IN A WHEELCHAIR FROM A 60 IN. x 60 IN. LEVEL LANDING AREA. A LEVEL LANDING AREA IS AN AREA WITH A CROSS SLOPE OF LESS THAN OR EQUAL TO 2%.
- THE 10 FT. MINIMUM SEPARATION BETWEEN PUSHBUTTONS IS TO BE MEASURED FROM FACE OF PUSHBUTTON TO FACE OF PUSHBUTTON, NOT CENTER OF POLE TO CENTER OF POLE.
- PUSHBUTTON ARROWS ARE TO BE TURNED PARALLEL TO THE CROSSWALK FOR WHICH THEY ARE INTENDED.
- ALL ACCESSIBLE PEDESTRIAN CONTROL EQUIPMENT SHALL BE DELIVERED TO THE SHA SIGNAL SHOP FOR TESTING AND PROGRAMMING PRIOR TO INSTALLATION. CONTACT MR. EDWARD RODENHIZER AT 410-787-7650 TO COORDINATE.
- ALL TRAFFIC SIGNAL MODIFICATIONS SHALL BE CONSTRUCTED PRIOR TO SIDEWALK CONSTRUCTION.
- THE CONTRACTOR SHALL ENSURE THE EXISTING TRAFFIC SIGNAL REMAINS OPERATIONAL UNTIL MODIFIED TRAFFIC SIGNAL IS OPERATIONAL.
- THE CONTRACTOR MUST COORDINATE THE INSTALLATION OF ALL PROPOSED SIGNAL EQUIPMENT WITH THE CONSTRUCTION OF THE PROPOSED SIDEWALK RAMP. SEE DRAWING NOS. DE-03 & DE-04 FOR SIDEWALK RAMP DETAILS. ALL WORK MUST BE COORDINATED WITH THE ROADWAY CONTRACTOR.

GENERAL NOTES

- MAINTENANCE OF TRAFFIC WILL BE HANDLED BY THE CONTRACTOR UTILIZING MSHA STANDARD TYPICALS FOR TRAFFIC CONTROL.
- THE CONTRACTOR SHALL CONTACT MISS UTILITY TO VERIFY ALL UNDERGROUND UTILITIES PRIOR TO THE INSTALLATION OF PROPOSED SIGNAL EQUIPMENT. IF ANY UTILITY CONFLICTS ARISE THE CONTRACTOR SHALL CONTACT THE PROJECT ENGINEER.
- WITHIN 36 IN. OF UNDERGROUND UTILITY LOCATIONS, THE CONTRACTOR SHALL BE REQUIRED TO EXCAVATE FOR FOUNDATION AND CONDUIT BY HAND.
- ALL TRAFFIC SIGNAL FOUNDATIONS SHALL BE INSTALLED AT THE FINAL SIDEWALK OR CURB GRADE FOR CLOSED SECTIONS. HIGHEST ROADWAY PROFILE GRADE FOR OPEN SECTIONS, TO MEET CLEARANCES AS SPECIFIED IN MD 816.03, MD 818.01, MD 818.02, MD 818.04. THE CONTRACTOR SHALL VERIFY ULTIMATE GRADES PRIOR TO THE INSTALLATION OF ALL SIGNAL EQUIPMENT.
- THE CONTRACTOR SHALL INTEGRATE PROPOSED/EXISTING CONCRETE FOUNDATIONS WITH NEW CURB/SIDEWALK RAMP WHERE NECESSARY. THE FOUNDATIONS SHALL BE FLUSH WITH AND PART OF THE FINAL CURB OR SIDEWALK GRADE TO INCREASE ACCESSIBILITY FOR PEDESTRIANS.
- THE SHA SIGNAL SHOP WILL BE RESPONSIBLE FOR ALL INTERNAL CABINET WIRING. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ROUTING AND PROPERLY LABELING ALL SIGNAL CABLES.
- THE CONTRACTOR SHALL VERIFY THE PROPOSED POLE LOCATION PRIOR TO INSTALLATION.
- THE CONTRACTOR SHALL CENTER THE PROPOSED CROSSWALKS ON NEWLY CONSTRUCTED RAMP.
- ALL PAVEMENT MARKINGS SHALL BE INSTALLED IN ACCORDANCE WITH MSHA STANDARDS.
- THE CONTRACTOR SHALL REFER TO FIGURE 3B-20a OF THE 2011 MARYLAND M.U.T.C.D. FOR CROSSWALK PAVEMENT MARKING DETAIL.
- VIDEO CAMERA LOCATION/ALIGNING SHALL BE COORDINATED WITH THE SHA ENGINEER.
- LOCATION OF ACCESSIBLE PEDESTRIAN SIGNAL PUSHBUTTONS MUST MEET LOCATION REQUIREMENTS OF MUTCD SEC. 4E.09 AND FIG. 4E.2; AND THE NCHRP PUBLICATION, "ACCESSIBLE PEDESTRIAN SIGNALS: GUIDE TO BEST PRACTICE." IF NOT MET, THE CONTRACTOR IS TO STOP WORK ON PUSHBUTTON LOCATIONS UNTIL THE CONFLICT HAS BEEN RESOLVED. IF NEEDED, A DESIGN WAIVER SHALL BE OBTAINED, APPROVED BY THE DIRECTOR, OFFICE OF TRAFFIC AND SAFETY.
- PUSHBUTTON IS TO BE LOCATED SO THAT A PEDESTRIAN IN A WHEELCHAIR LOCATED ON THE LEVEL LANDING AREA DOES NOT HAVE TO REACH MORE THAN 18 IN.
- VIDEO DETECTION CAMERAS 'a' AND 'b' SHALL BE PROGRAMMED FOR PRESENCE AND SAMPLING DETECTION ALONG MERRITT BOULEVARD.



STATE OF MARYLAND
DEPARTMENT OF TRANSPORTATION
STATE HIGHWAY ADMINISTRATION
OFFICE OF TRAFFIC & SAFETY
TRAFFIC ENGINEERING DESIGN DIVISION
MD 157 (MERRITT BOULEVARD)
AND SEARLES ROAD
DUNDALK, MARYLAND

TRAFFIC SIGNALIZATION PLAN

SCALE 1" = 20' DATE 8/2004 CONTRACT NO. AT3675185

DESIGNED BY M. HOWELL COUNTY BALTIMORE

DRAWN BY LOGWILE 03015703.22

CHECKED BY TIMS NO. J639

F.A.P. NO. TOD NO.

TS NO. 4340B DRAWING SG-05 OF 08 SHEET NO. 70 OF 100



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UTILITY LEGEND

E — E — ELECTRIC CABLES — S — S — STORM DRAIN
A — A — AERIAL CABLES — G — G — GAS MAIN
T — T — TELEPHONE CABLES — W — W — WATER MAIN
F — F — FIBER-OPTIC — S — S — SEWER MAIN

GEOMETRIC LEGEND

— — — — PROPOSED
— — — — EXISTING

APPROVALS
REVISIONS
TEAM LEADER
ASST. DIR. CHIEF
DIVISION CHIEF
OFFICE DIRECTOR

INSTALL APCPS FOR NORTH, EAST AND WEST
SHA NO. BA3325177 TIMES # L009 03/2013
STV
A REPLACE SIGNAL HEADS WITH LED
SHA NO. BA1161668 TIMES # J639 09/2009
KMP RB WH TH

PLOTTED: Thursday, February 07, 2013 AT 04:20 PM
FILE: I:\PROJECTS\4013887\4013887_0016\Drawings\TRA\Traffic\Signal\Searles\005_p5G-P003_MD157_Searles_Road.dgn